



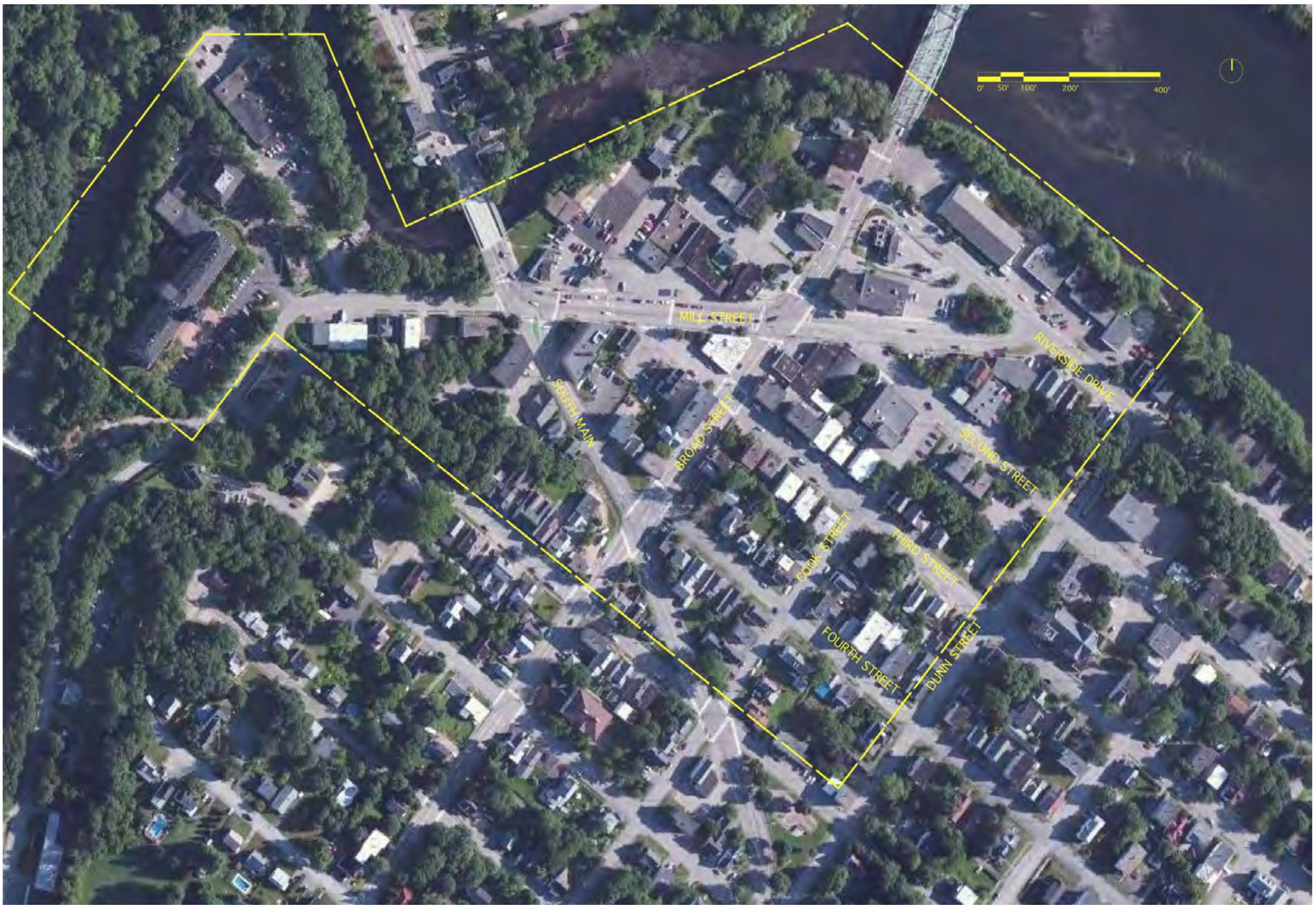
New Auburn Village Center Study

Council Meeting

28 July 2014

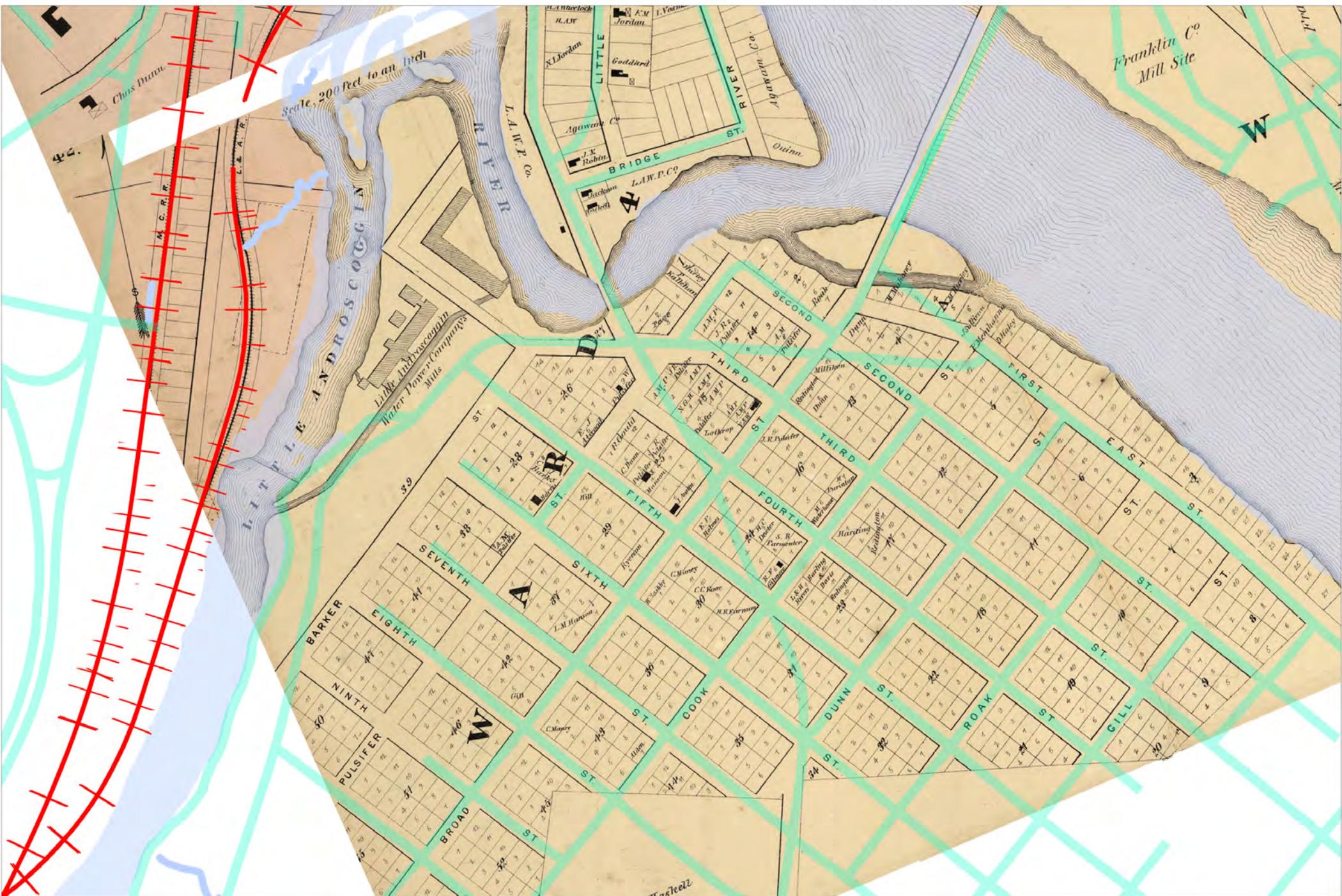
1. Process
2. Analysis
3. Concepts
4. Community Consensus
5. ROW Improvements
6. Urban Design
7. Phasing
8. Probable Costs
9. Implementation Strategies

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38-Acre Study Area

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1873 Plat (with existing streets in green)

PROCESS

Value and Purpose Statement – CSS Process

New Auburn Village Center is a thriving, walkable community. It is a place, a destination, a gateway and an inviting entrance to downtown Auburn. The compact center is safe, inviting and home to restaurants, shops, markets and services. Strategic investments in the public realm such as parking, streetscapes, transportation improvements and the expanded Little Androscoggin Park demonstrate an ongoing commitment on behalf of the City to make smart choices that have exponential returns.

The placemaking opportunities of economic development are carefully considered when proactively working with the private sector. It is understood that each development or redevelopment is part of a bigger picture of revitalizing New Auburn Village Center as a neighborhood of urban streets, buildings and public spaces. By making a place inviting to people, a place is made that is inviting to businesses and attractive for new investment.

Over the past several decades, neighborhood businesses including restaurants, shops, a bank and a hardware store have closed their doors. It is evident that the tendency to build public infrastructure favoring pass through commuters and the automobile at the expense of pedestrian needs has played a role in the changing economic conditions and will need to be considered if renewed investment is desired. The New Auburn Village Center Study will be a success when residents see the role the past can play in a new economy and change is measured by increased community vitality, new investment and growing property values.

Process

- Review the existing transportation network, urban form and land use in the New Auburn Center District;
- Determine effectiveness of the network in light of proposed land-use changes and build-out scenarios;
- Identify and evaluate vehicle, bicycle, pedestrian, transit, and parking transportation alternatives that include both physical improvements as well as potential changes to City ordinances and development standards;
- Work with the Advisory Committee, as well as local community businesses and residents on recommendations;
- Develop recommendations for the network to improve traffic flow while respecting and improving the traditional form of New Auburn, and;
- Develop cost estimates and prioritized list of improvements/recommendations, which may include project phasing.

Steering Committee and Support Staff

- Leroy Walker: Ward 5 City Councilor
- Ken Blais: New Auburn Village Business, Representative of the United New Auburn Association
- John Roy: Firehouse Grille, New Auburn Village Business
- Tina Croteau: Marcel's Barber Shop, New Auburn Village Business
- Eric Potvin: New Auburn Village Real Estate/Developer
- Larry Pelletier: New Auburn Village Resident
- Mia Poliquin-Pross: Planning Board Member
- Eric Cousens: Director of Planning and Permitting, City of Auburn
- Doug Greene: City Planner, City of Auburn
- Dan Goyette: City Engineer / Deputy Public Works Director, City Auburn
- Jason Ready: Transportation Planner, ATRC

Steering Committee and and Public Meetings

- Steering Committee: May 30, 2013
- Public Meeting #1: June 20, 2013
- Steering Committee: July 11, 2013
- Steering Committee: August 15, 2013
- Public Meeting #2: September 5, 2014
- Steering Committee: October 10, 2013
- Steering Committee: October 24, 2013
- Steering Committee: November 21, 2013
- Steering Committee: December 19, 2013
- Public Meeting #3: February 6, 2014
- Steering Committee: April 10, 2014

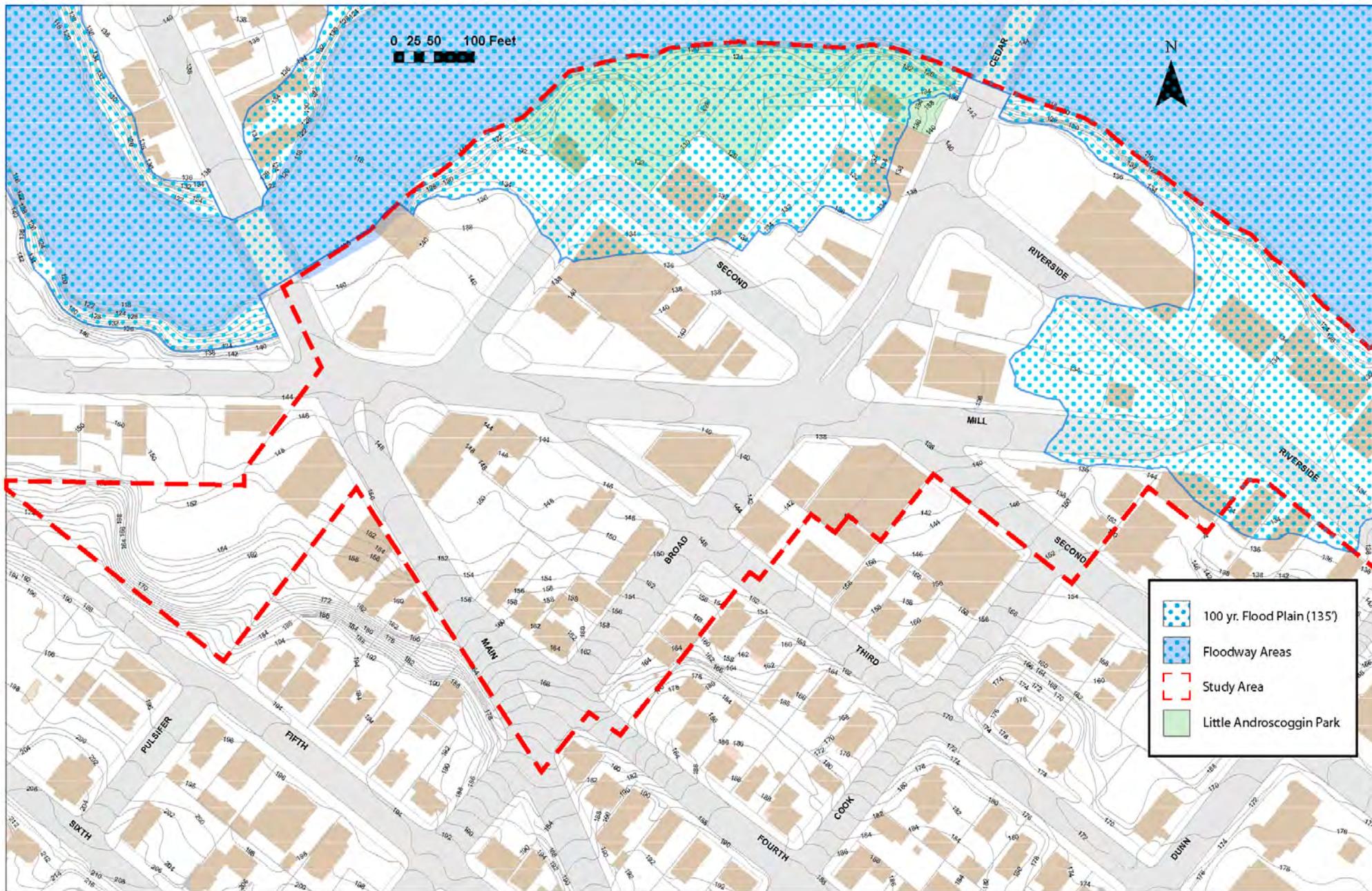
ANALYSIS

Issues Reviewed

- Existing Traffic Volumes
- Projected Traffic Volumes
- Traffic Patterns
- Crash Data
- Intersection Operations
- Inventory of Sidewalks, Crosswalks and Signs
- Historic Development Patterns
- Current Development Trends
- Land Uses
- Building Forms
- Parking
- Open Space
- Streetscape
- Connectivity
- Topography
- Flood Plain
- Redevelopment Opportunities
- Previous Studies / Current Zoning and Standards

CONCEPTS

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NEW AUBURN VILLAGE CENTER STUDY

OPTION 1 CONCEPT

10 OCTOBER 2013

TY LIN INTERNATIONAL
MLRD LANDSCAPE ARCHITECTURE + URBANISM

KEY:

100-YEAR FLOODPLAIN: 

TWO FLOOR BUILDING: 

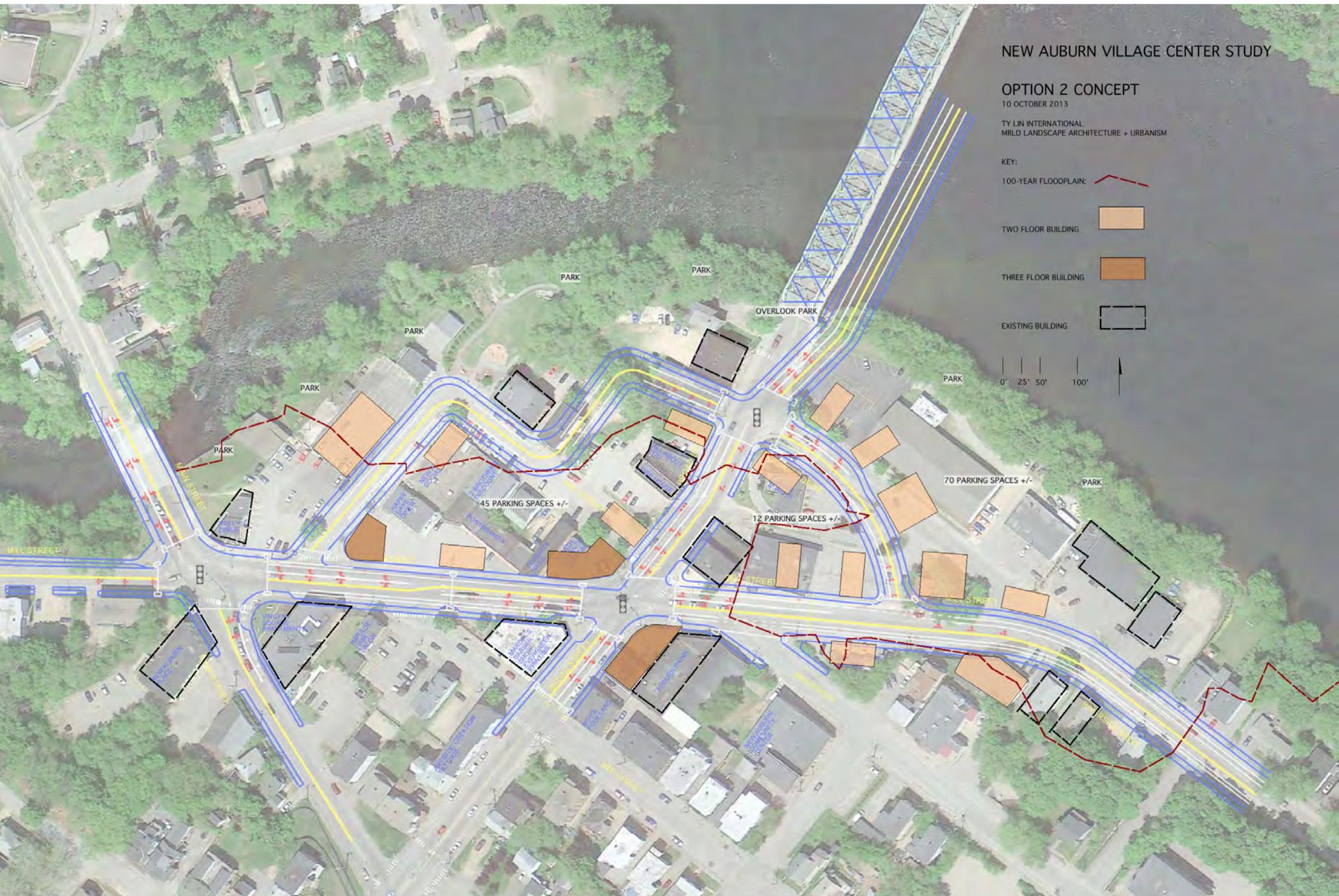
THREE FLOOR BUILDING: 

EXISTING BUILDING: 

0' 25' 50' 100'



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NEW AUBURN VILLAGE CENTER STUDY

OPTION 2 CONCEPT

10 OCTOBER 2013

TY LIN INTERNATIONAL
MLRD LANDSCAPE ARCHITECTURE + URBANISM

KEY:

100-YEAR FLOODPLAIN: 

TWO FLOOR BUILDING: 

THREE FLOOR BUILDING: 

EXISTING BUILDING: 

0' 25' 50' 100'



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	Existing Conditions	One-Way Loop	Grid	Option 1 Variation	Option 1	Option 2	Riverway
<u>Metrics</u>							
Study Area	18 Acres +/-	18 Acres +/-	18 Acres +/-	18 Acres +/-	15 Acres +/-	15 Acres +/-	18 Acres +/-
Existing Residential Units	101 Units +/-	65 Units +/-	59 Units +/-	55 Units +/-	49 +/-	55 Units +/-	57 Units +/-
Proposed Residential Units	0	150 Units +/-	186 Units +/-	191 Units +/-	82 +/-	54 Units +/-	192 Units +/-
Total Residential	101 +/-	215 Units +/-	245 Units +/-	246 Units +/-	131 +/-	109 Units +/-	249 Units +/-
Existing Commercial	85,048 SF +/-	15,579 SF +/-	15, 579 SF +/-	15,579 SF +/-	15,867 SF +/-	24,953 SF +/-	17,149 SF +/-
Proposed Commercial		90,000 SF +/-	111,600 SF +/-	118,800 SF +/-	76,065 SF +/-	63,225 SF +/-	115,200 SF +/-
Total Commercial	85,048 SF +/-	105,579 SF +/-	127, 179 SF +/-	134,379 SF +/-	91,932 SF +/-	88,178 SF +/-	132,349 SF +/-
100-Year Flood Plain	5.5 Acres +/-	5.5 Acres +/-	5.5 Acres +/-	5.5 Acres +/-	5.5 Acres +/-	5.5 Acres +/-	5.5 Acres +/-
ROW	5 Acres +/-	5 Acres +/-	4.40 Acres +/-	4.5 Acres +/-	4 Acres +/-	4.40 Acres +/-	4.50 Acres +/-
Riverfront Park	1.2 Acres +/-	1.2 Acres +/-	1.2 Acres +/-	1.5 Acres +/-	1.55 Acres +/-	1.4 Acres +/-	1.7 Acres +/-
Net Residential Density	9 Units Per Acre +/-	18.22 Units Per Acre +/-	19.97 Units Per Acre +/-	20.50 Units Per Acre +/-	14 Units Per Acre +/-	12 Units Per Acre +/-	21 Units Per Acre +/-
On-Street Parking	90 +/-	146 +/-	160 +/-	181 +/-	123 +/-	125 +/-	170 +/-
Off-Street Public Parking	14 +/-	0	0	0	14 +/-	14 +/-	0
Off-Street Private Parking	256 +/-	348 +/-	398 +/-	412 +/-	299 +/-	241 +/-	291 +/-
<i>Sub Total Existing Parking</i>	<i>360 +/-</i>	<i>494 +/-</i>	<i>558 +/-</i>	<i>593 +/-</i>	<i>436 +/-</i>	<i>380 +/-</i>	<i>461 +/-</i>
Total Required Parking by Uses	425 +/-	674 +/-	791 +/-	816 +/-	502 +/-	458 +/-	814 +/-
Total Required w/ 1/3 Shared Efficiency	280 +/-	452 +/-	530 +/-	547 +/-	337 +/-	307 +/-	546 +/-

COMMUNITY CONSENSUS

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Public Meeting #3 | 2.6.14 | Barker Mill Arms

Feedback On Key Master Plan Issues

- Relocating the Bridge - **YES**
- Placing Buildings Close to Street - **YES**
- Height of Buildings - **3 TO 4 FLOORS**
- Shared Parking Areas Behind Buildings - **YES**
- On-Street Parking - **YES**
- Two-Way Traffic Versus One-Way - **TWO-WAY**
- Traffic Signals Versus Roundabouts - **SIGNALS, BUT REVIEW**
- Closing South Main Between Broad and Cook - **REVIEW**
- Encouraging Streets with Different Character - **YES**
- Making the Riverfront Public Space - **YES**
- Designing to Maximize Place Versus Accommodating Traffic - **YES**

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NEW AUBURN VILLAGE CENTER STUDY

MRLD / TY LIN

PROPOSED THREE STORY BUILDINGS



EXISTING BUILDINGS



SCALE: 1" = 50'



2014 Master Plan

ROW IMPROVEMENTS

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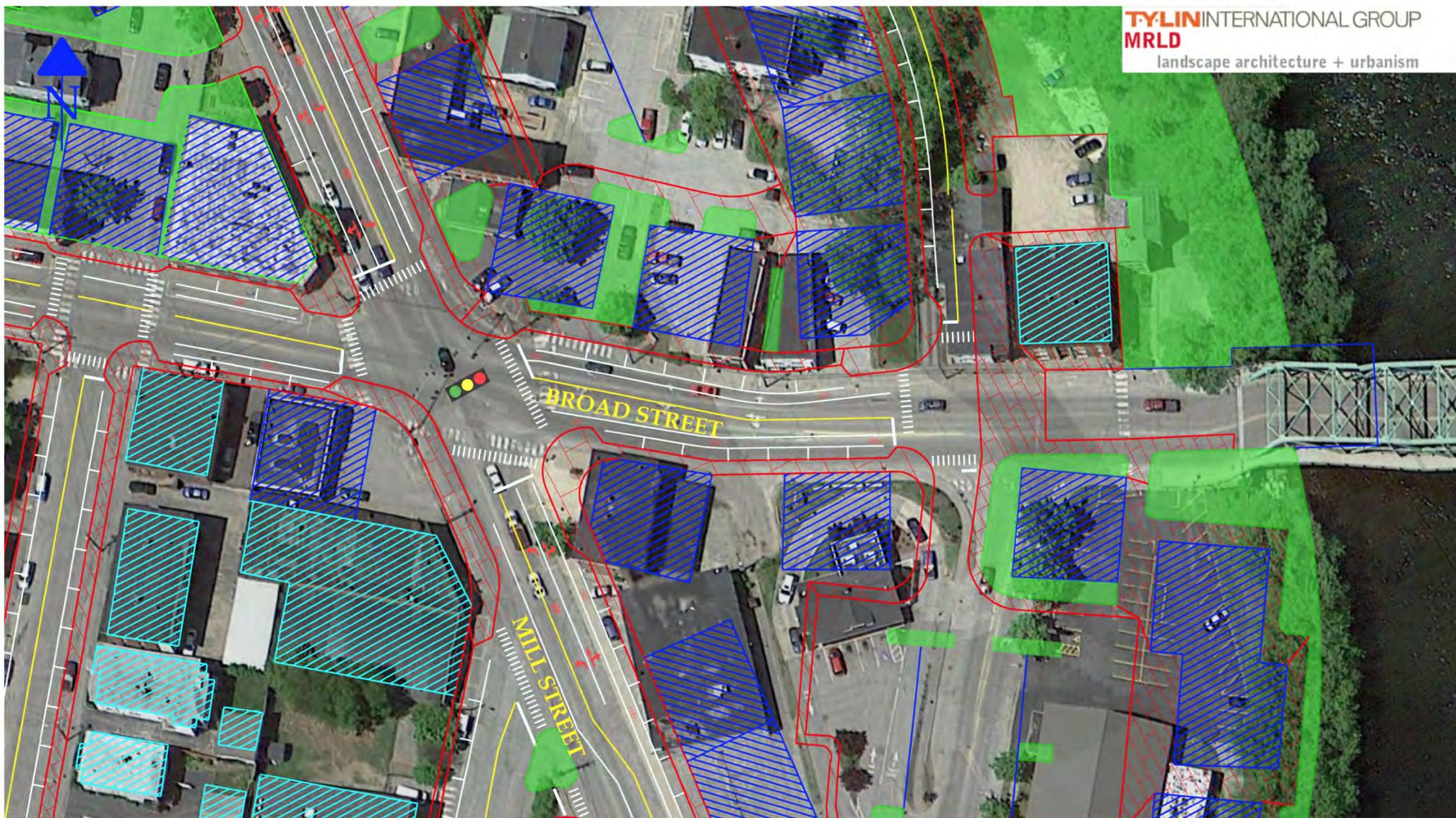


NEW AUBURN VILLAGE STUDY
Figure 1 of 5 : Conceptual Roadway Improvements



DRAFT

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TYLIN INTERNATIONAL GROUP
MRLD
landscape architecture + urbanism

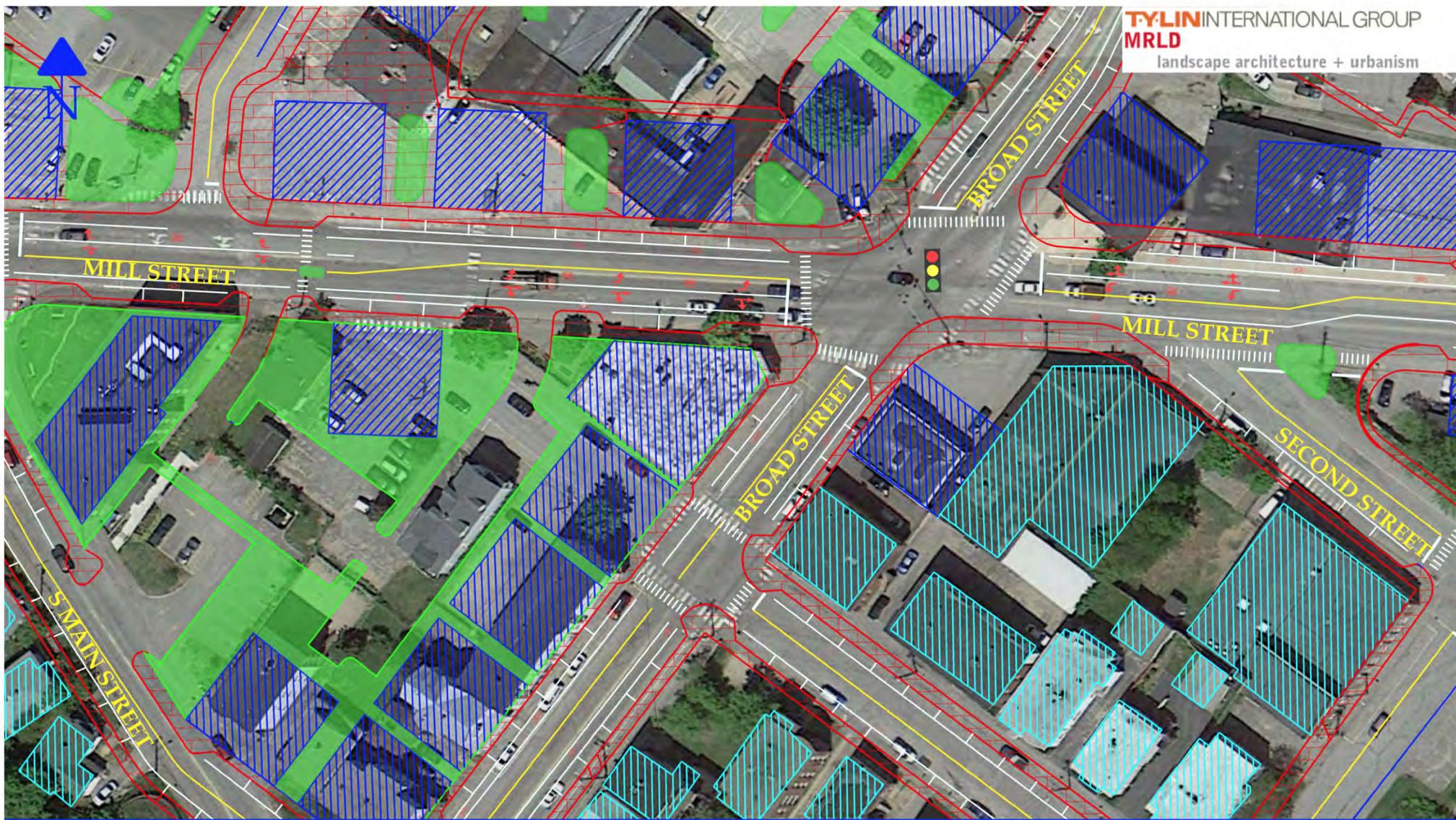
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Figure 2 of 5 : Conceptual Roadway Improvements



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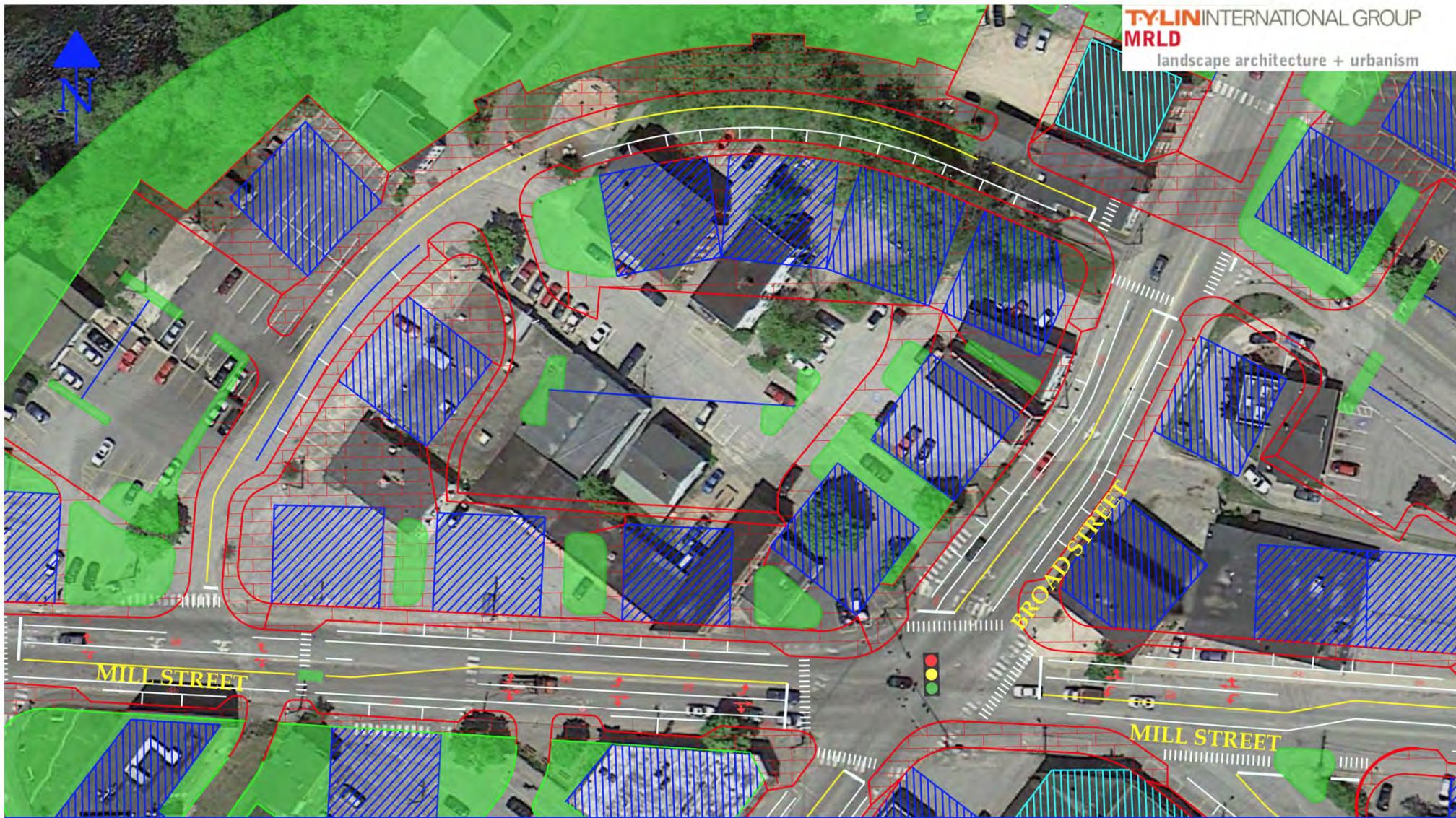


NEW AUBURN VILLAGE STUDY
Figure 3 of 5 : Conceptual Roadway Improvements

0' 50' 100'
SCALE: 1"=50'

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MILL STREET

BROAD STREET

MILL STREET

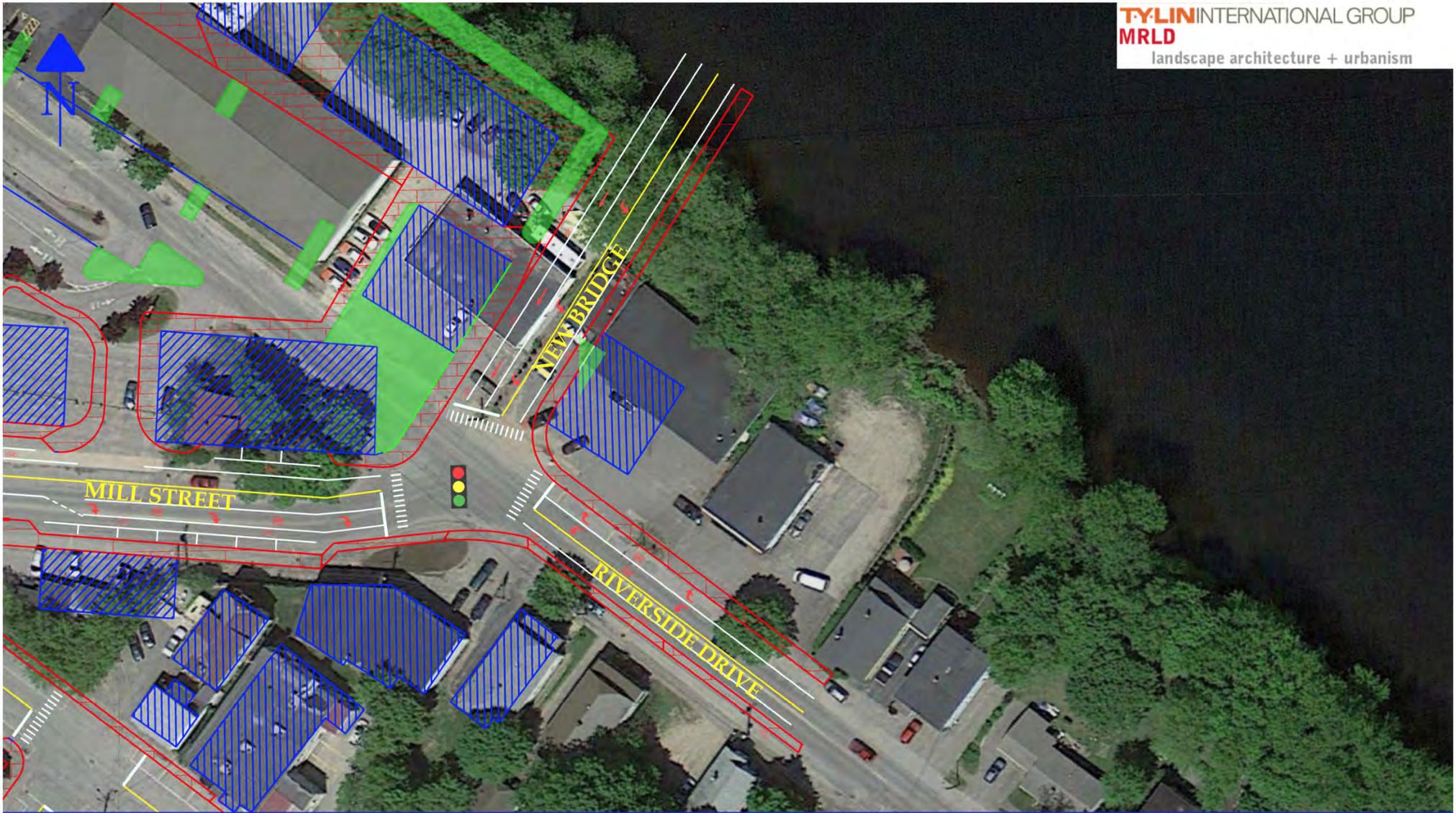
NEW AUBURN VILLAGE STUDY

0' 50' 100'

DRAFT

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NEW AUBURN VILLAGE STUDY
Figure 5 of 5 : Conceptual Roadway Improvements



DRAFT

URBAN DESIGN

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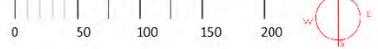


NEW AUBURN VILLAGE CENTER STUDY

MRLD / TY LIN

- PROPOSED THREE STORY BUILDINGS 
- EXISTING BUILDINGS 

SCALE: 1" = 50'



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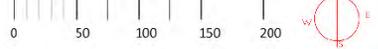
NEW AUBURN VILLAGE CENTER STUDY

MRLD / TY LIN

PROPOSED THREE STORY BUILDINGS 

EXISTING BUILDINGS 

SCALE: 1" = 50'



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- Relocating the Lown Bridge to the Mill Street / Riverside Drive intersection. This routes traffic on Mill and allows for the new “Riverway” and “Androscoggin” blocks to evolve as quiet, dense neighborhoods that take advantage of views and access to the Little Androscoggin and Androscoggin Rivers;
- Closing Riverside Drive between Mill and Broad to create an internal parking lot allowing for perimeter development;
- Closing Second Street between Broad and Mill (and creating the Riverway) to create an internal parking lot allowing for perimeter development;
- Closing Third between Broad and Mill creating infill development opportunities as well as off-street parking;
- Constructing an esplanade along the Riverway overlooking the Little Andy Park. This esplanade and steps will be on fill that will create a defined edge to the flood plain, creating new redevelopment opportunities above the 100-year flood plain elevation;

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- Continuing the Little Andy Park to a “Broad Street Plaza” — a new civic space and overlook at the location of the existing bridge;
- Locating a Riverwalk between the Androscoggin Block buildings and the Androscoggin River to allow for public access as well as economic development opportunities;
- Placing buildings close to the sidewalk and street in a traditional manner;
- Creating vibrant and safe streetscapes with wide sidewalks, street trees, pedestrian-scaled lighting, bike racks and other amenities, and;
- Complying with Auburn’s Complete Street policy
- Making all streets two-way
- Increasing residential density in the focus area from 9 +/- units per acre to 18-21 +/- units per acre
- Accepting a lower level of service in terms of traffic in order to create a place that better serves the needs of New Auburn

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New Bridge Intersection – Mill and Riverside

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Mill Street

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Broad Street at 3rd

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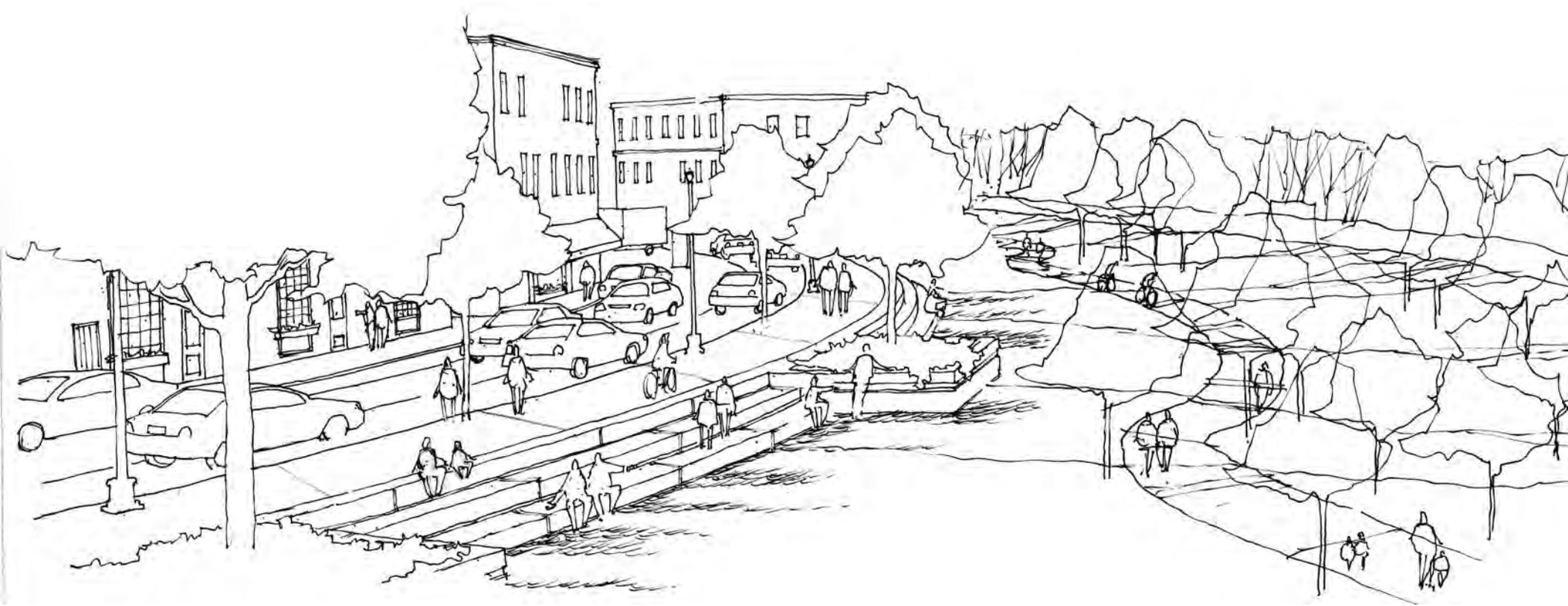


Broad Street Plaza

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Riverway – Existing Conditions



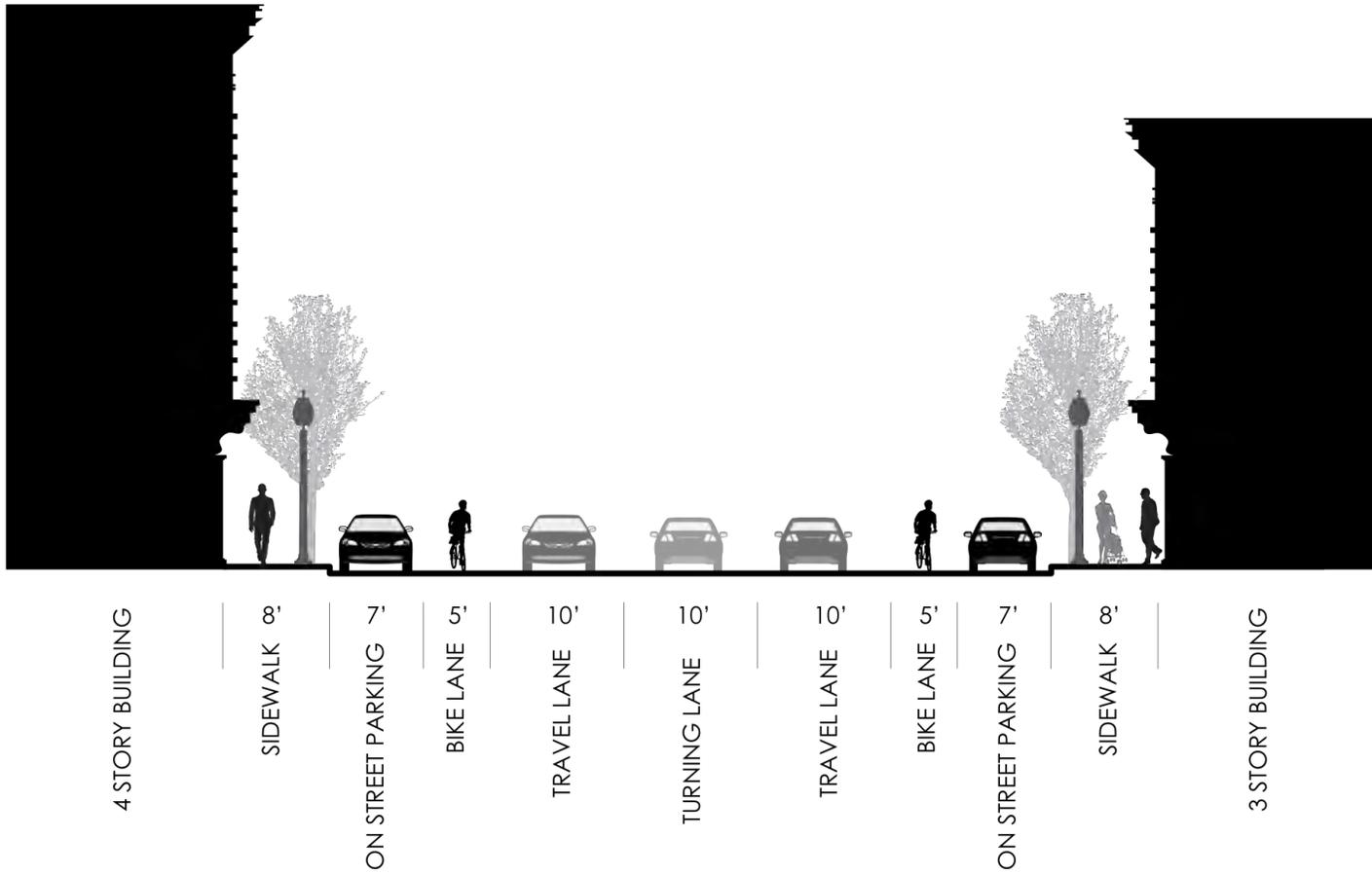
Riverway – Proposed

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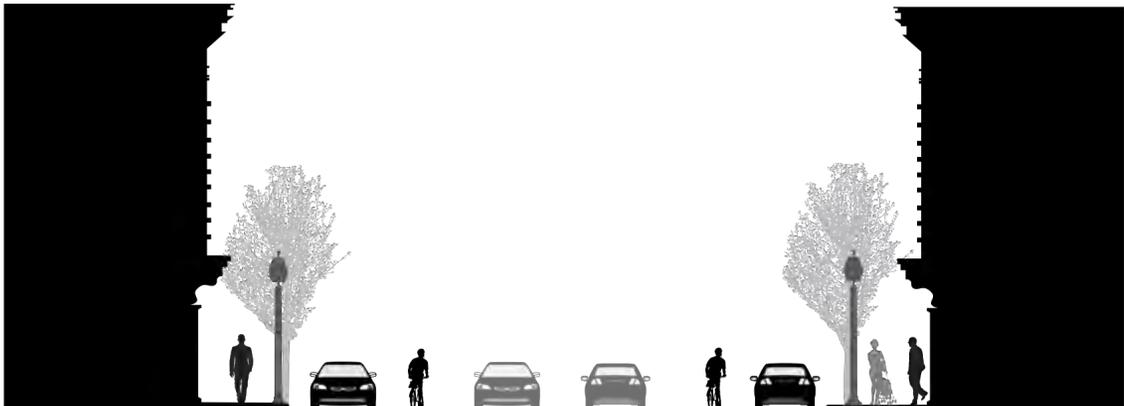
Androscoggin Block

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Mill Street

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4 STORY BUILDING

SIDEWALK 8'

ON STREET PARKING 7'

BIKE LANE 5'

TRAVEL LANE 10'

TRAVEL LANE 10'

BIKE LANE 5'

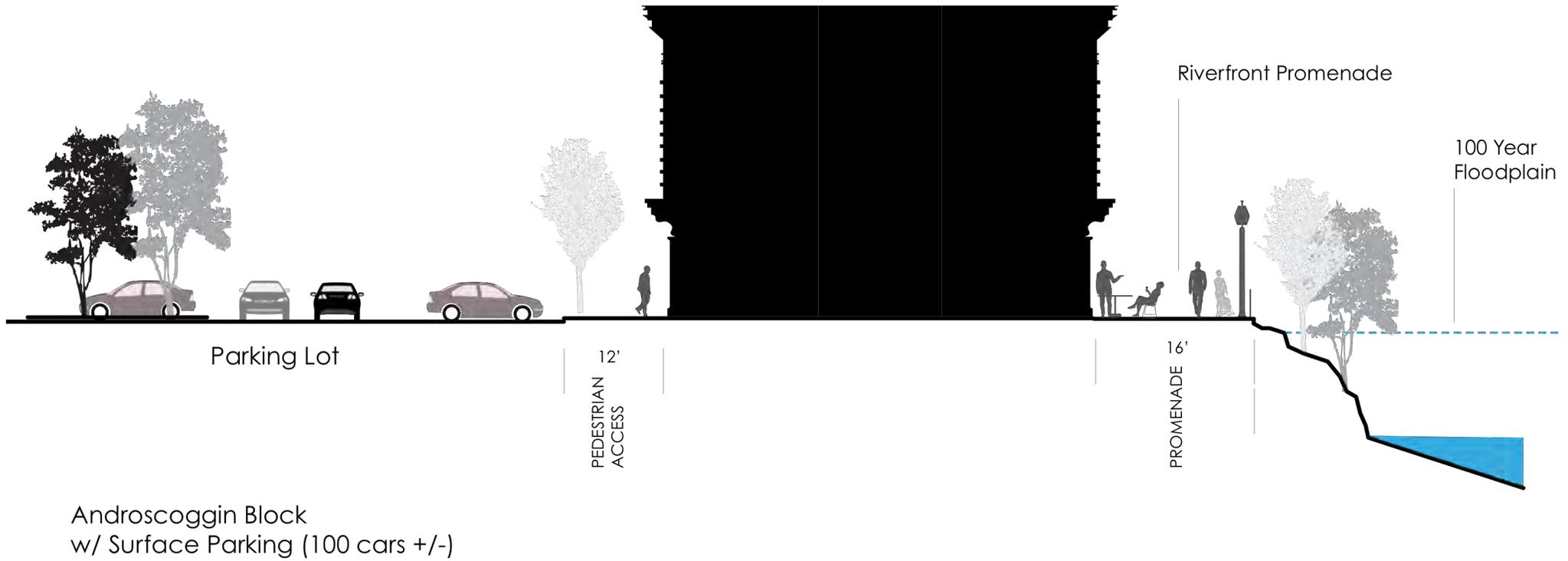
ON STREET PARKING 7'

SIDEWALK 8'

3 STORY BUILDING

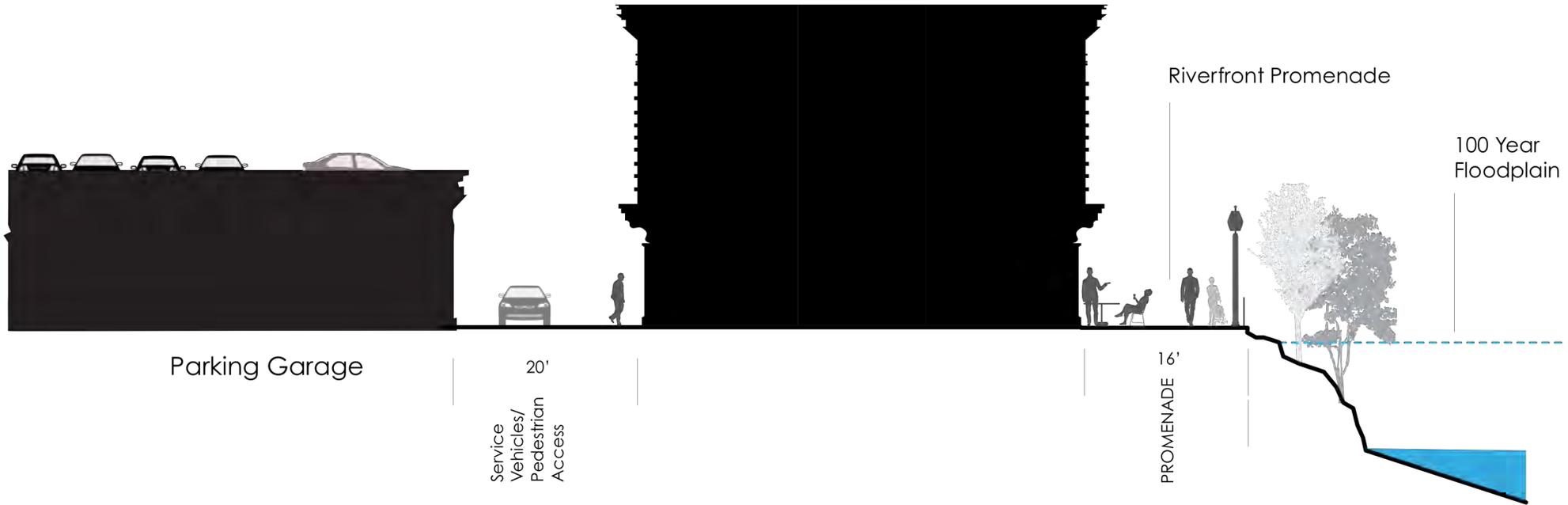
Broad Street

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Androscoggin Block – w/ Surface Parking

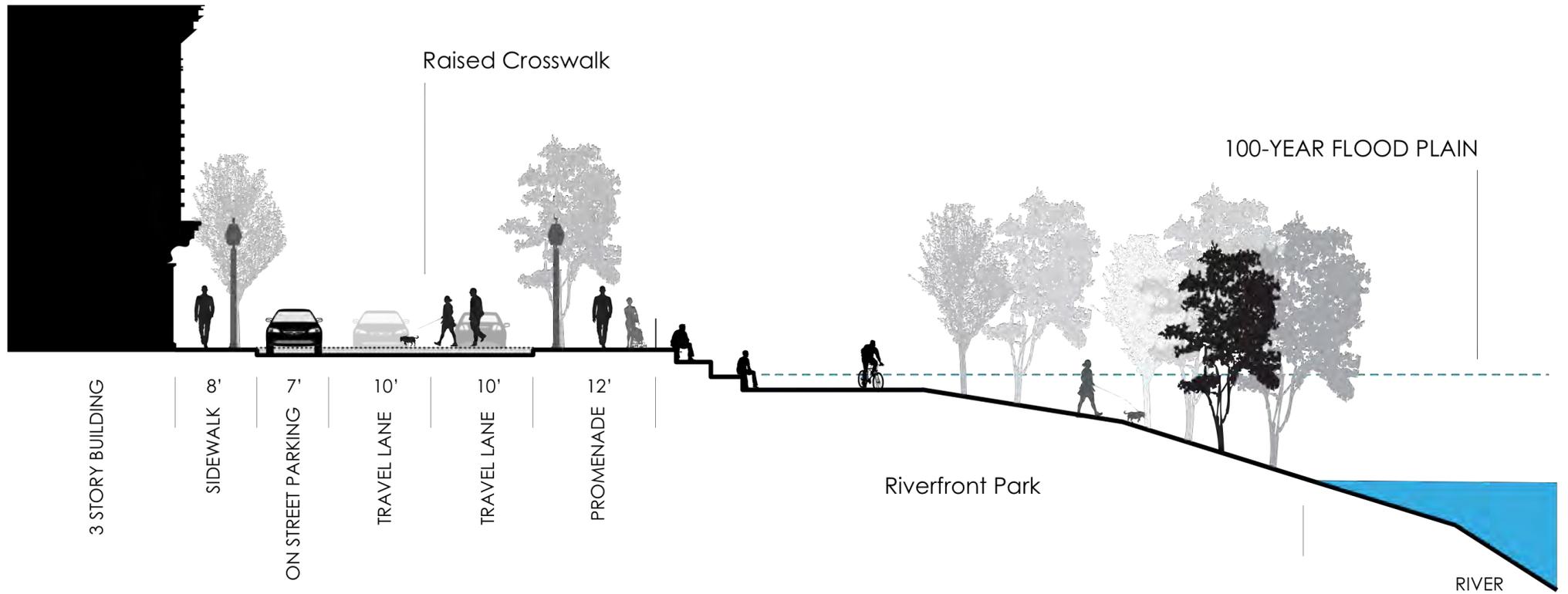
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Androscoggin Block
w/ Parking Garage (300 cars +/-)

Androscoggin Block – w/ Structured Parking

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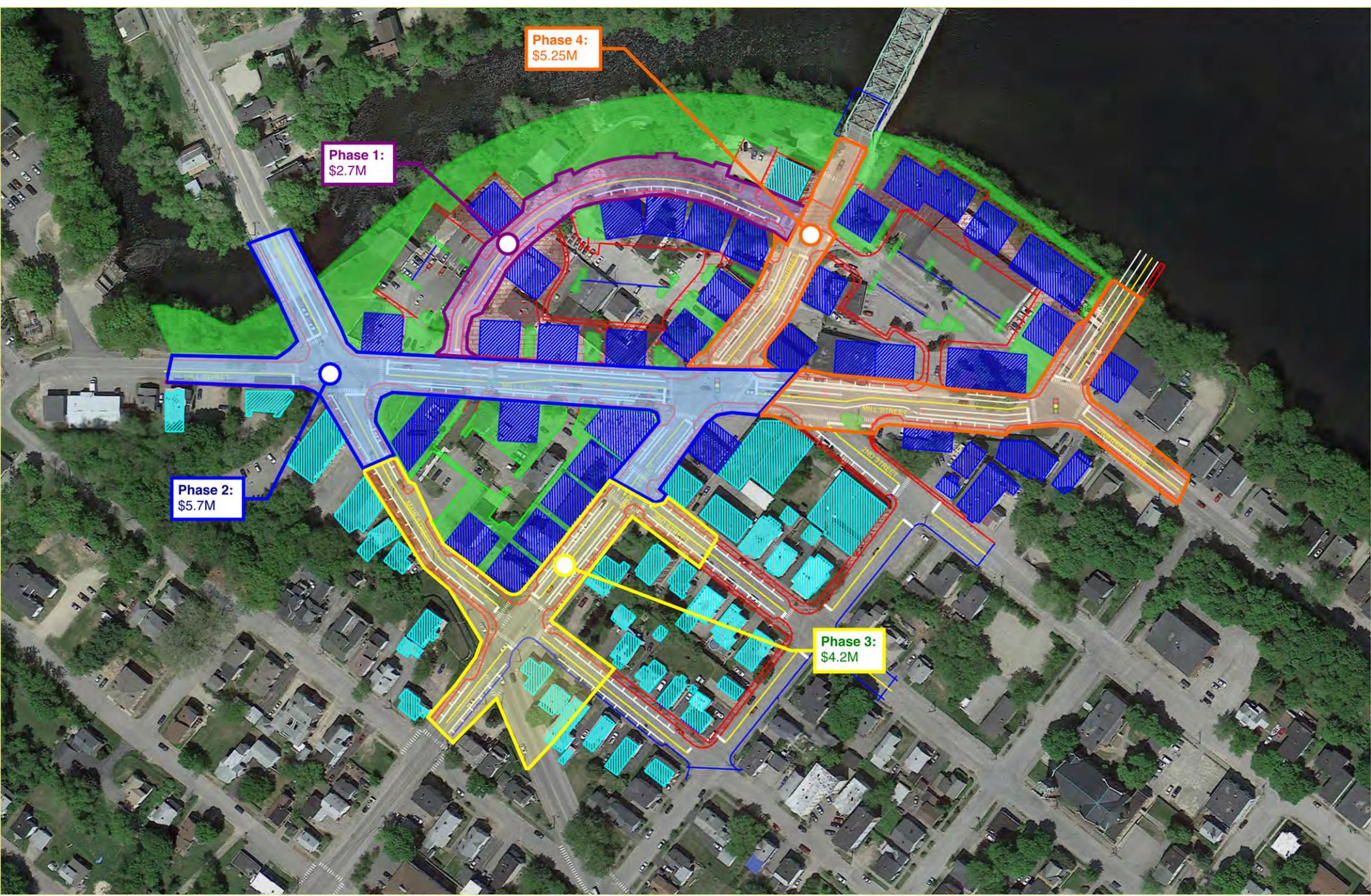
Riverway

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	Existing Conditions	Riverway
<u>Metrics</u>		
Study Area	18 Acres +/-	18 Acres +/-
Existing Residential Units	101 Units +/-	57 Units +/-
Proposed Residential Units	0	192 Units +/-
Total Residential	101 +/-	249 Units +/-
Existing Commercial	85,048 SF +/-	17,149 SF +/-
Proposed Commercial		115,200 SF +/-
Total Commercial	85,048 SF +/-	132,349 SF +/-
100-Year Flood Plain	5.5 Acres +/-	5.5 Acres +/-
ROW	5 Acres +/-	4.50 Acres +/-
Riverfront Park	1.2 Acres +/-	1.7 Acres +/-
Net Residential Density	9 Units Per Acre +/-	21 Units Per Acre +/-
On-Street Parking	90 +/-	170 +/-
Off-Street Public Parking	14 +/-	0
Off-Street Private Parking	256 +/-	291 +/-
<i>Sub Total Existing Parking</i>	<i>360 +/-</i>	<i>461 +/-</i>
Total Required Parking by Uses	425 +/-	814 +/-
Total Required w/ 1/3 Shared Efficiency	280 +/-	546 +/-

PHASING

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PROBABLE COSTS

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Phase 1: \$2.7 M

- Riverway from Mill to Broad 2.7M

Phase 2: \$5.7 M

- Mill Street, West of Main Street \$1.1M (This section assumes an overlay section of pavement.)
- Mill Street, Main to Broad St: \$2.3M (This section assumes a widening section and pavement rehabilitation.)
- Main Street, North of Mill Street: \$.800K (This section assumes an overlay section of pavement.)
- Main Street, Mill to Broad Street: \$900K (1/2)
- Broad Street, Main Street to Mill Street: \$600K (1/3)

Phase 3: \$4.2 M

- Main Street, Mill to Broad Street: \$900K (1/2)
- Fourth Street, from Broad to Cook St: \$1M
- Third Street, Cook Street to the End: \$300K
- Broad Street, Main Street to Mill Street: \$1.2M (2/3)
- Broad Street, South: \$.8M

Phase 4: \$5.25 M

- Mill Street, Broad to Riverside: \$2.2M (This section assumes a widening section and pavement rehabilitation.)
- Second Street to Cook Street: \$750k
- Riverside Street: \$1.1M
- Broad Street Mill to End: \$1.2M

The costs above do not assume any right of way costs, costs to construct the new bridge, removal of the old bridge, existing building demolition, or improvements to existing sites outside of the right of way. Included in the costs are common borrow costs, pedestrian lights, street trees and grates, traffic signal additions and updates, brick sidewalk with granite curb, drainage and sewer rehabilitation and adjustments, underground utilities, pavement markings, new signage, removal of existing pavement and other common excavation, maintenance of traffic, mobilization, contingency, design engineering, and construction administration.

IMPLEMENTATION STRATEGIES

Every development, infrastructure / transportation / open space improvement is an opportunity leading to the Master Plan vision.

Think holistically and strategically in terms of the overall character of the area, not the traditional lot-by-lot approach that has often resulted in suburban type development in urban areas.

Floodplain

- New development areas are established in the creation of the Riverway Block, Androscoggin Block and by relocating the Lown Bridge to the Mill / Riverside Intersection
- Base elevations are raised between six feet and one foot to a minimum required elevation of 136'
- Little Andy Park floods as needed
- Use flood tolerant materials and limit structures in park
- Permitting is easier
- Cost of insurance and construction reduced

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Leverage closed ROW's within Riverway, Androscoggin and South Gateway Blocks

- Create interior shared parking
- Maximize perimeter block development frontage
- Minimize curb cuts
- Maximize on-street parking
- Leverage public / private partnerships: land swaps, RFP's, etc.

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- PROPOSED THREE STORY BUILDINGS 
- EXISTING BUILDINGS 
- RIGHT-OF-WAY OPPORTUNITIES 

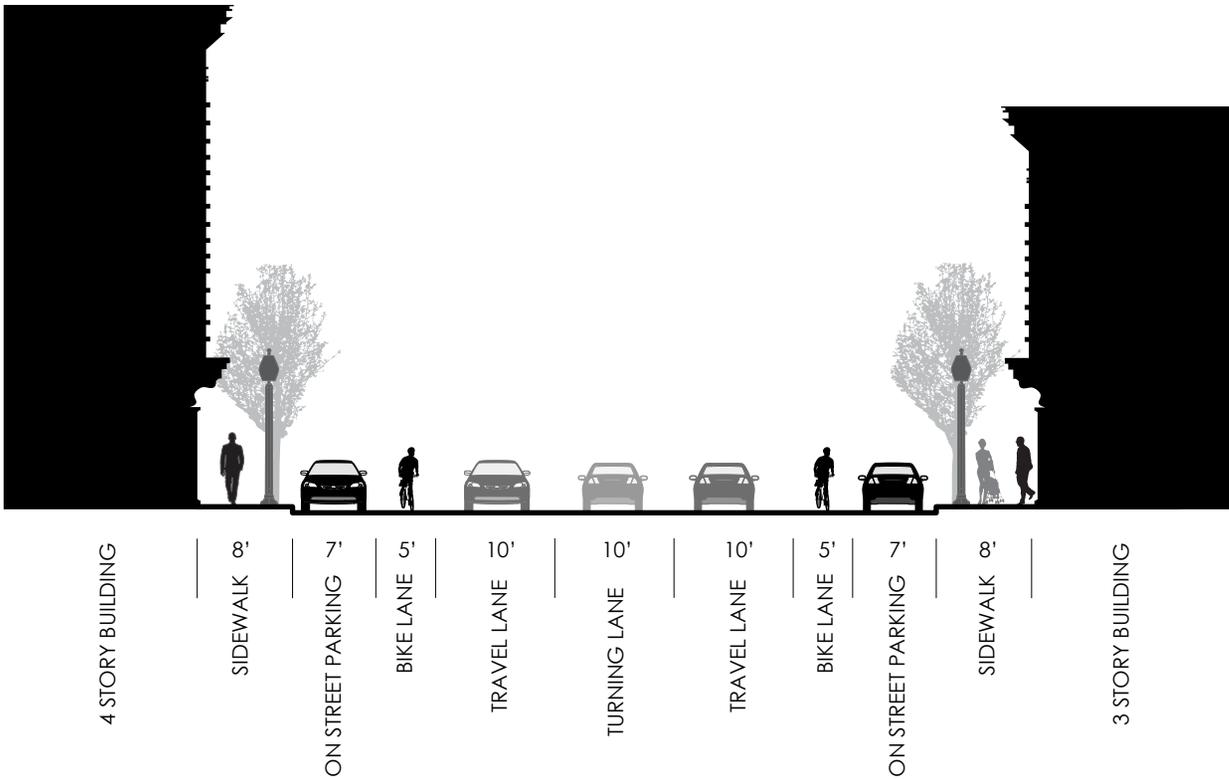
Zoning

- Continue with Form-Based Code process
- Short-term zoning changes:
 - Maximum footprint (can currently build 100,000 SF building, which is out of scale for urban setting)
 - Maximum setbacks (allow 0' front setbacks)
 - Allow zero lot line development
 - Increase allowable lot coverage (80% to 100%)
 - Building height – three to four floors
 - Revise required parking for uses (reduce required ratios)
 - No drive-thrus
 - Shared parking ordinance
 - Utilize residential dwelling uses for Multi-Family Urban rather than Multi-Family Suburban – more mixed use
 - No single-family homes or residential uses on first floor (unless grandfathered)
- Recommended edits to 2009 New Auburn Master Plan:
 - Place areas of Andrscoggin Block within in the proposed Limited Business Zone in New Auburn Village Center Zone
 - Place areas of the Riverway Block within the proposed Riverway Transition Zone in the New Auburn Village Center Zone

Building Form and Placement

- Focus on form, placement and how buildings address street frontage
- Both sides of the street should echo each other in terms of standards, helping to shape the “outdoor room”

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Streetscape / Complete Streets

- Complete Street standards on all streets
- ADA compliant
- Wide and safe sidewalks
- Street trees, streetlights and other amenities
- Streets as civic space defined by proper form and placement of buildings
- Bury utilities

Parking

- Distribute parking evenly throughout focus area
- Maximize on-street parking
- Create interior shared parking lots
- Establish a shared parking ordinance
- Phase structured parking if needed
- Master Plan includes:
 - 170 +/- on-street spaces
 - 291 +/- shared spaces

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Open Space

Leverage different types of open space for quality of life, neighborhood character and to jumpstart economic development:

- Riverway
- Little Andy Park
- Androscoggin Walk
- Broad Street Plaza
- Pocket Parks
- Streets
- Connections to greater sidewalk and trail systems in New Auburn / Auburn / Lewiston

View Corridors / View Sheds

View corridors and views sheds take advantage of the grid, the topography and the river frontage:

- Riverway and Androscoggin Blocks establish / protect views to Little Androscoggin and Androscoggin
- Existing grid south of Mill Street in combination with topography frames views to north and northwest
- Views from new location of Lown Bridge back to redeveloped downtown will help reinforce identity of the community

Connectivity

A higher level of connectivity makes the area easier to redevelop, safer for pedestrians and mitigates traffic impacts:

- Two-way traffic circulation
- Do not limit turning movements
- Connectivity outside focus area is maintained (intersections, block lengths, pedestrian shed) *Note – whole 38-acre study area has 23 intersections and the United States Green Building Council's Leadership in Energy and Environmental Design requires at least 8 intersections for a comparable area*
- Connectivity within focus area is enhanced with high accessibility, two-way traffic, streetscape and intersection improvements
- Pedestrian orientation of the Riverway / Little Andy Park and the Androscoggin Walk adds to connectivity
- Safe pedestrian crossings at improved intersections and at select mid-block locations encourages foot traffic and economic development

Housing

- Target to increase residential density in 18-acre focus area from 9 units per acre to 18-21 units per acre

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NEW AUBURN VILLAGE CENTER STUDY

MRLD / TY LIN

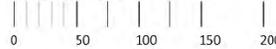
PROPOSED THREE STORY BUILDINGS



EXISTING BUILDINGS



SCALE: 1" = 50'



2014 Master Plan