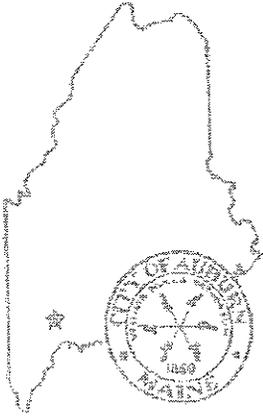


# Application for Drive Entrance

Fee: \$20.00



City of Auburn, Maine  
Maine's City of Opportunity  
Community Services Department  
60 Court Street, Suite 114  
Auburn, ME 04210  
(207)333-6600 - Voice  
(207)333-6601 - Automated  
(207)333-6625 - Fax  
www.auburnmaine.org

For Office Use Only
Drive Entrance Permit #:
PID#:
Issue Date:
Excavation Permit #:
Culvert Size:

Property Owner's Name (Please Print): \_\_\_\_\_ Date Applied: \_\_\_\_\_

Mailing Address: \_\_\_\_\_

Phone #: \_\_\_\_\_

Application to construct / modify an Entrance located on or at: \_\_\_\_\_, at a point about \_\_\_\_\_ Feet (North) (South) (East) (West) from \_\_\_\_\_ (Utility Pole # / Street / Driveway / etc.) for the purpose of entering a: Residence ( ) Gas Station ( ) Restaurant ( ) Industry ( ).

The Following is information regarding the location and requested entrance (s):

1. Frontage of lot along street \_\_\_\_\_ Feet
2. Number of entrances requested \_\_\_\_\_
3. Proposed width of entrance (s) \_\_\_\_\_
4. The finished surface of the proposed driveway is to be: \_\_\_\_\_
5. Construction to begin on or about \_\_\_\_\_, and expected to be completed \_\_\_\_\_
6. Is the entrance part of a project / development requiring a D.E.P. site location permit?  YES  NO

### The Applicant Hereby Agrees:

1. To provide, erect and maintain all necessary barricades, lights, warning signs and other devices to safeguard traffic properly while the work is in progress.
2. That the street will at no time be closed to traffic.
3. Where the drive is located in a curb, and/or sidewalk section, the applicant will remove the curb and transport it to a storage site selected by the City of Auburn.
4. To construct and maintain said driveway approach in accordance with Maine Department of Transportation specifications as adopted by the City of Auburn (see attached).

A further condition of the permit shall be that the applicant shall well and truly pay all damages, fines, and penalties for which he/she shall become liable, and shall indemnify and save harmless the City of Auburn against all suits, claims, damages and proceeding of every kind arising out of the construction and maintenance of said driveway approach, including snow removal within the City of Auburn right-of-way.

\_\_\_\_\_  
Signature of Authorizing Applicant / Agent

\_\_\_\_\_  
Date

Show proposed entrance locations, width and arrangement; distance between entrances; setback of buildings, gasoline pumps, etc., in relation to the centerline of the traveled way; length, size and location of existing pipes, culverts, catchbasins or manholes, curbing, gutter, and/or sidewalks, and the proposed location of new pipes, culverts, catchbasins or manholes, curbing, etc.

**Specific Conditions:**

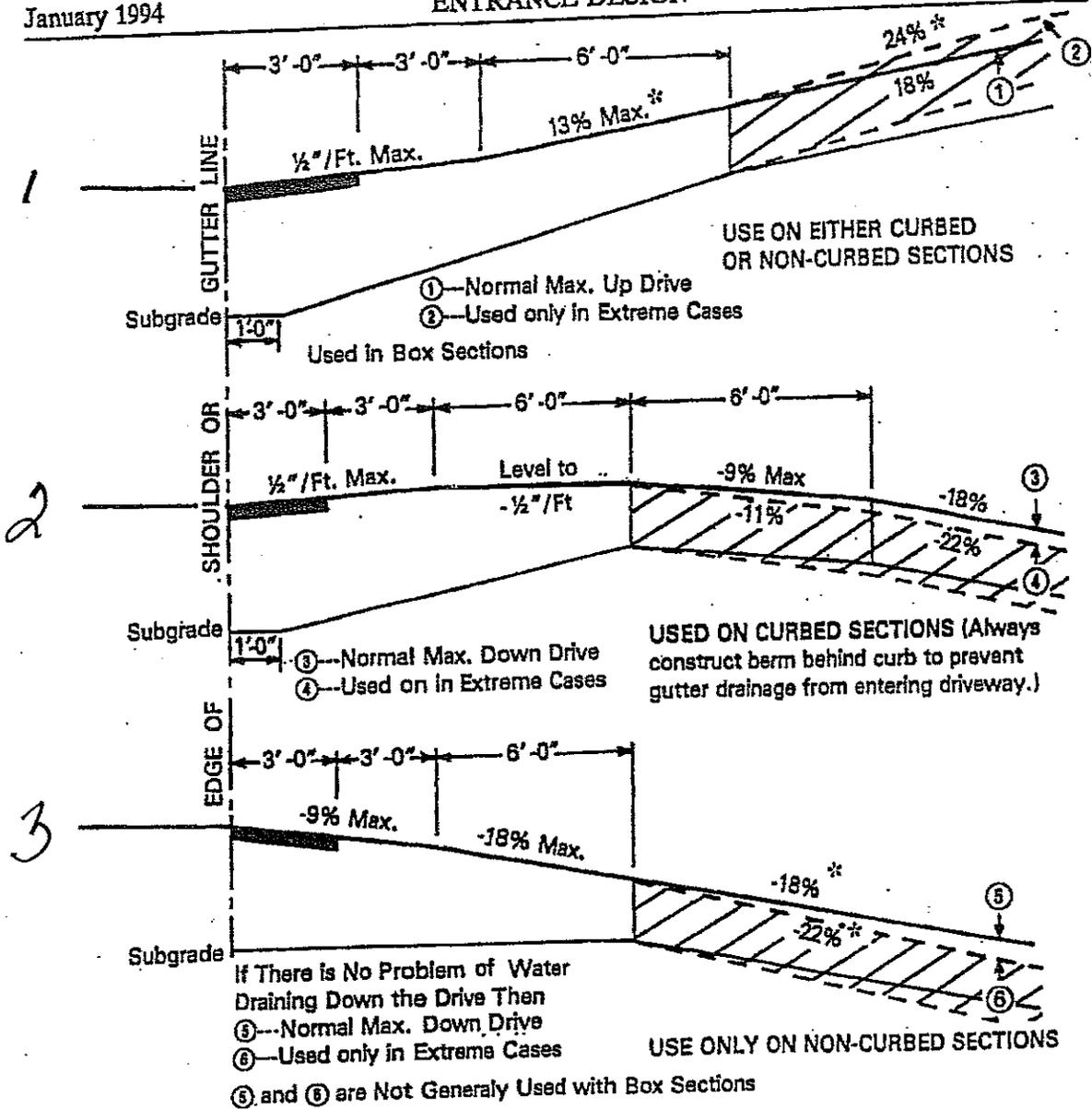
Must follow City of Auburn driveway design constructions standards: for second entrance (minimum 75' of frontage required) 20' opening on each entrance maximum width allowed. Install a 3' paved apron from edge of road pavement. Stabilize all material used for drive construction from migration into City Road or drainage ditch. (Note: Contractor to secure a Public Property Excavation Permit BEFORE any work takes place in th City Right of Way).

Must follow City of Auburn driveway design construction standards: for one entrance 32' maximum width allowed. Install a 3' paved apron from edge of road pavement. Stabilize all material used for drive construction from migration into City Road or drainage ditch. (Note: Contractor to secure a Public Property Excavation Permit BEFORE any work takes plave in the City Right of Way).

\_\_\_\_\_  
**Approved By**

\_\_\_\_\_  
**Date**

Conditions of Approval: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_



**GENERAL NOTES:**

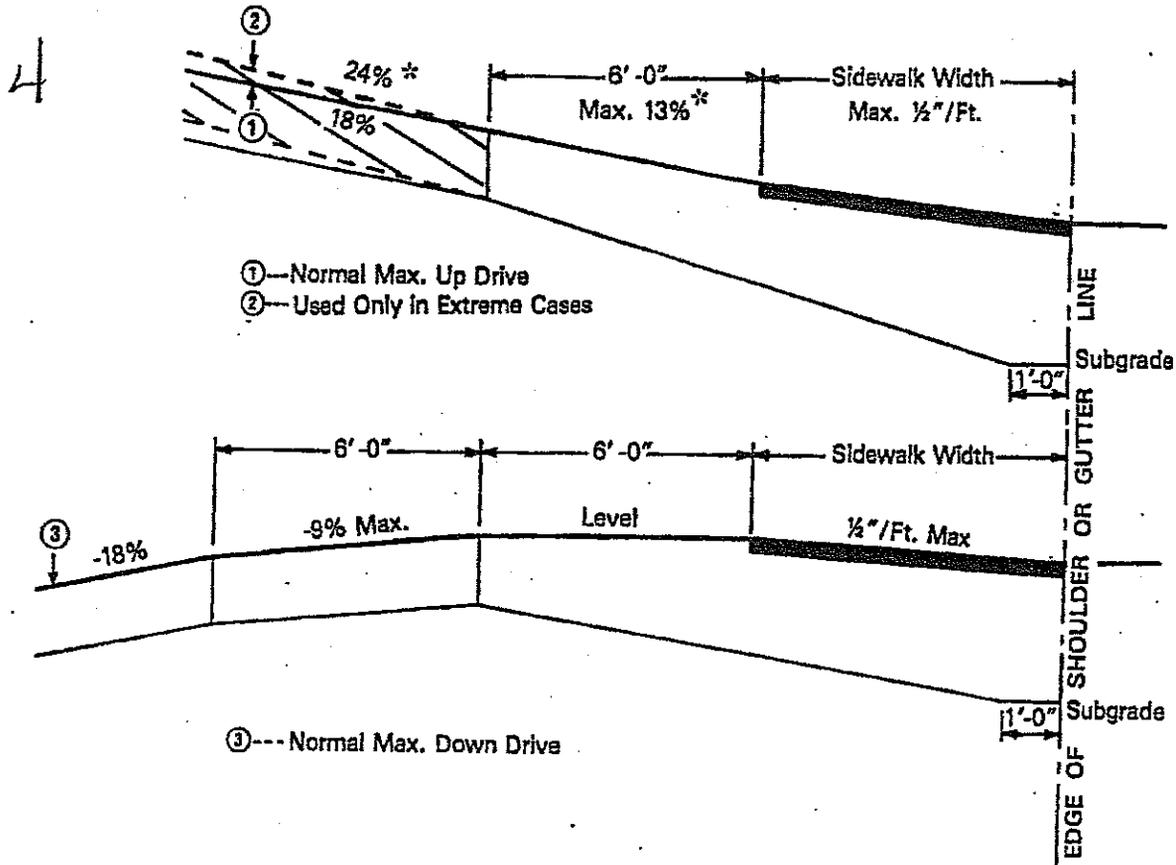
- ① The first 3' shown as pavement will be paved only when abutting a paved shoulder to a non-paved entrance.
- ② All residential or commercial drives 10% and over will be paved.

**NOTES ON MAXIMUM DRIVEWAY PROFILES:**

- ① These profiles are a guide for the majority of cases but should be field checked when the main line grade is steep (4% to 6% or greater) or the angle of approach to the drive is unusual.
- ② Generally, the majority of drives on a project will be built with flatter profiles than these maximum cases.
- ③ When grading drives which are flatter than the maximum profiles, the following rule of thumb should be used: Do not exceed a grade % change of more than 9% in a 6-foot increment of driveway length. This applies to both up and down profiles.

**TYPICAL ENTRANCE PROFILE  
(No Sidewalk)**

Figure 8-27

**GENERAL NOTES:**

- ① The sidewalk width will be paved in all cases.
- ② All residential or commercial drives 10% and over will be paved.

**NOTES ON MAXIMUM DRIVEWAY PROFILES:**

- ① These profiles are a guide for the majority of cases but should be field checked when the main line grade is steep (4% to 6% or greater) or the angle of approach to the drive is unusual.
- ② Generally, the majority of drives on a project will be built with flatter profiles than these maximums.
- ③ When grading drives which are flatter than the maximum profiles, the following rule of thumb should be used: Do not exceed a grade % change of more than 9% in a 6-foot increment of driveway length. This applies to both up and down profiles.

**TYPICAL ENTRANCE PROFILE**  
**(Sidewalks)**

Figure 8-28